

# MEMORANDUM

November 5<sup>th</sup>, 2014

**TO:** Landmarks Board

**FROM:** Lesli Ellis, Comprehensive Planning Manager  
James Hewat, Senior Historic Preservation Planner  
Marcy Cameron, Historic Preservation Planner  
Angela Smelker, Historic Preservation Intern

**SUBJECT:** Public hearing and consideration of a Landmark Alteration Certificate to construct a 300 sq. ft. addition and 150 sq. ft. screened-in mechanical area and restore the trackside arched openings at the Union Pacific Depot at 30<sup>th</sup> and Pearl St., and individual landmark, per section 9-11-18 of the Boulder Revised Code (HIS2014-00299).

## STATISTICS:

1. Site: Northeast of 30<sup>th</sup> St. and Pearl Pkwy
2. Designation: Individual Landmark
3. Historic Name(s): Boulder Jaycees Depot
4. Date of Construction: 1890
5. Zoning: MU-4
6. Applicant: James Bray, Bray Architecture, Inc.

## STAFF RECOMMENDATION:

If the applicant complies with the conditions listed below, the proposed addition will be generally consistent with the conditions specified in Section 9-11-18, B.R.C. 1981, the *General Design Guidelines*, Staff recommends that the Landmarks Board adopt the following motion:

*The Landmarks Board adopts the staff memorandum dated November 5, 2014 in matter 5C (HIS2014-00299) as the findings of the board and approves the construction of a 300 sq. ft. addition and 150 sq. ft. screened-in mechanical area at the north and east elevations of the Union Pacific Depot as shown on plans dated 10/10/2014, finding that they generally meet the standards for issuance of a Landmark Alteration Certificate in Chapter 9-11-18, B.R.C. 1981, subject to the following conditions:*

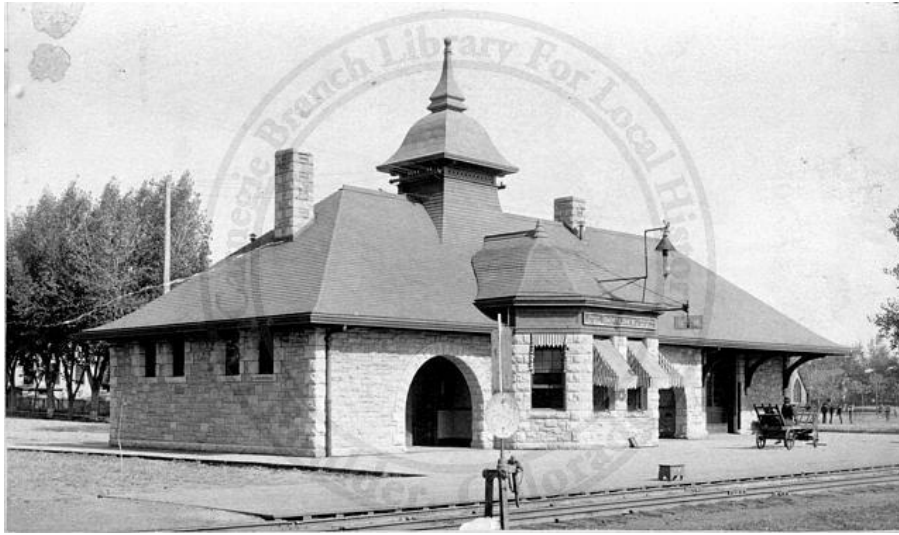
## CONDITIONS OF APPROVAL:

1. The applicant shall be responsible for constructing the addition and reconstruction of the alcoves in compliance with the approved plans dated 10.10.2014, except as modified by these conditions of approval.
2. Prior to submitting a building permit application and final issuance of the Landmark Alteration Certificate, the applicant shall submit revised drawings for review and approval by the Ldrc that show the trackside arch walls inset to a minimum depth of 2' from the exterior wall, and fenestration inside the arched openings to more closely match that shown in historic drawings.
3. Final details showing door and window details, roofing materials, wall materials and proposed colors. These design details shall be reviewed and approved by the Landmarks design review committee, prior to the issuance of a building permit. The applicant shall demonstrate that the design details are in compliance with the intent of this approval and the *General Design Guidelines*.

## SUMMARY:

- In 2007, a Landmark Alteration Certificate was approved for the relocation and stabilization of the Depot. The depot was relocated northeast of 30<sup>th</sup> and Pearl Street on the street now known as Junction Place.
- On September 25, 2014, an application was submitted for the construction of a screened mechanical and service entry following referral of the addition to the full Landmarks by the Landmark design review committee.
- After meeting with staff to review the proposal, the applicants submitted revised plans on Oct. 10, 2014.
- Staff finds the proposed addition to be generally consistent with the criteria for a Landmark Alteration Certificate as per 9-11-18(a) & (b)(1)-(4) B.R.C. 1981, the *General Design Guidelines*.
- Staff's recommendation to approve the proposed addition is based upon the understanding that final details will be reviewed and approved by the Landmarks design review committee (Ldrc) prior to the issuance of a Landmark Alteration Certificate.

## PROPERTY HISTORY:



*Figure 1: Boulder Depot shortly after construction at 14<sup>th</sup> and Canyon Blvd., c.1890.*

The Union Pacific Depot was constructed in 1890 on the north side of Canyon Boulevard and 14<sup>th</sup> Street. It operated until 1957, when rail transport ceased to downtown Boulder. Until 1973, the building was used as a bus terminal and travel agency. In 1973, under threat of demolition by the City of Boulder, the Boulder Jaycees purchased the building and moved it to the Pow Wow Rodeo Grounds near 30<sup>th</sup> and Pearl streets.



*Figures 2. The depot being transported in two pieces to its new location, at 2275 30<sup>th</sup> St., 1973.*



*Figure 3. 2007 view of the Depot while located at 2275 30<sup>th</sup> St.*

The Jaycees used the facility for their offices and as a meeting space. In order to offset the costs of maintenance for the depot, the Jaycees also rented out the space on a short term basis for private uses, such as wedding receptions, parties, and business meetings. This worked fairly well for the organization for many years. However, overhead and maintenance costs grew at a fast rate. As a non-profit organization with limited resources, the Jaycees were not able to afford upkeep of the Depot. The City of Boulder purchased the depot in 2007.

Due to development interest in the area, the city relocated the Depot a second time east across 30<sup>th</sup> Street to the site of Boulder's planned Transit Village in 2008. Several years of planning went into the move of the building including substantial structural reinforcement of the walls with carbon fiber.

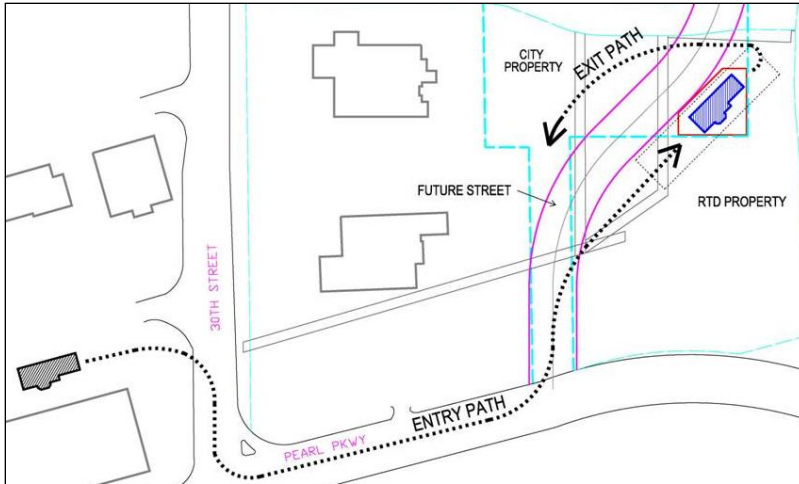


Figure 4. Map of Depot's second relocation, 2007.



Figure 5. The Depot at its 3<sup>rd</sup> and current location northeast of 30<sup>th</sup> and Pearl, 2014.

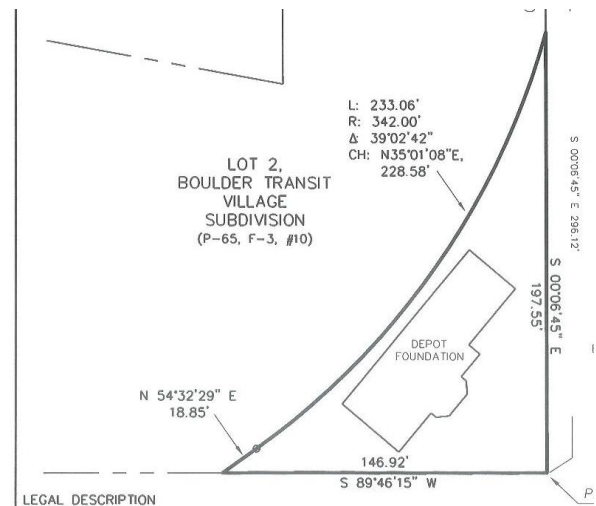


Figure 6: Location Map (left) and Landmark Boundary (right).



**DESCRIPTION:**

The property is located north of Pearl Parkway and east of 30<sup>th</sup> Street in the Boulder Junction development, which is currently under construction. The 1890 Union Pacific Depot is a classic example of a western Victorian train station. Built in the Romanesque Revival style, the depot's most prominent features include a rough-cut stone exterior, quoins, arched entrances, hipped roof, and a decorative cupola.

The Landmark boundary for the depot is a roughly triangular shaped piece of land in Junction Place that is owned by the City of Boulder. It's unusual configuration owes to the original land lot that was owned by the city's Housing Department and to the fact that the depot was relocated to this location in 2008, prior to planning for Junction Place having been completed. However, considerable consideration was given to ensuring that the building would have a relationship to the original Union Pacific rail line, still in operation and located to the east of the depot. The landmark designation was amended as part of the move in October of 2007. Since the move, development of the larger Junction Place site has proceeded. When complete the area will have a hotel, a number of housing units, a park and plaza area, a wonerf and bridge over Goode Creek, and will function as the Regional Transit Department's Rapid transit center. In the planning for Junction Place, the depot has treated as a central component to the development with consideration given to referencing its historic context as a transportation hub in Boulder from the 1980s until the late 1960s.

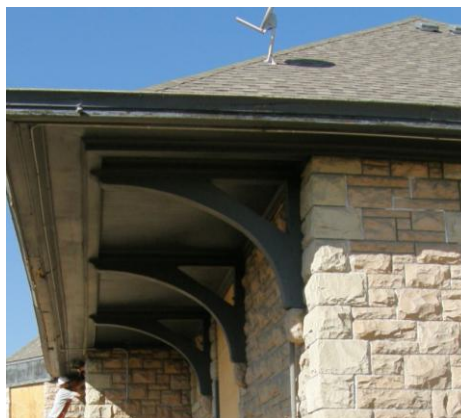


*Figure 7. Detail of window (non-historic) and stone surround, 2006.*

*Figure 8. Detail of bay window, 2006.*



*Figure 9. View of the interior of the depot, 2013.*



*Figure 10. Detail of quoins, overhanging roof, and brackets, 2008.*

### **PROPOSED REHABILITATION AND ADDITION TO HISTORIC DEPOT**

The City of Boulder has entered into a long-time lease agreement with the developer of Junction Place who is responsible for the rehabilitation of the interior of the building and restoration of the exterior including construction of the front porte-cochere, roof and tower elements and trackside arched openings (formerly men's and women's entrances to segregated waiting areas in the building. This work is based upon the original drawings for the depot and has been reviewed and approved by the Ldrc.

In order facilitate re-use of the depot as a restaurant, the applicant proposes an enclosed service entry of 300 square feet and a screened mechanical area of an additional 150 square feet to be constructed on the northeast corner of the building. A previous Landmark Alteration Certificate for the depot approved the installation of an exterior staircase to the basement at the north side of the building. However, the applicant indicates this configuration will not work for the proposed restaurant circulation and that the area needs to be enclosed in order to meet health code and to be usable during the winter months. The proposed restaurant also requires a mechanical service area. The applicant proposes to enclose this area with a screen in order to prevent visual exposure that would detract from the character of the building.

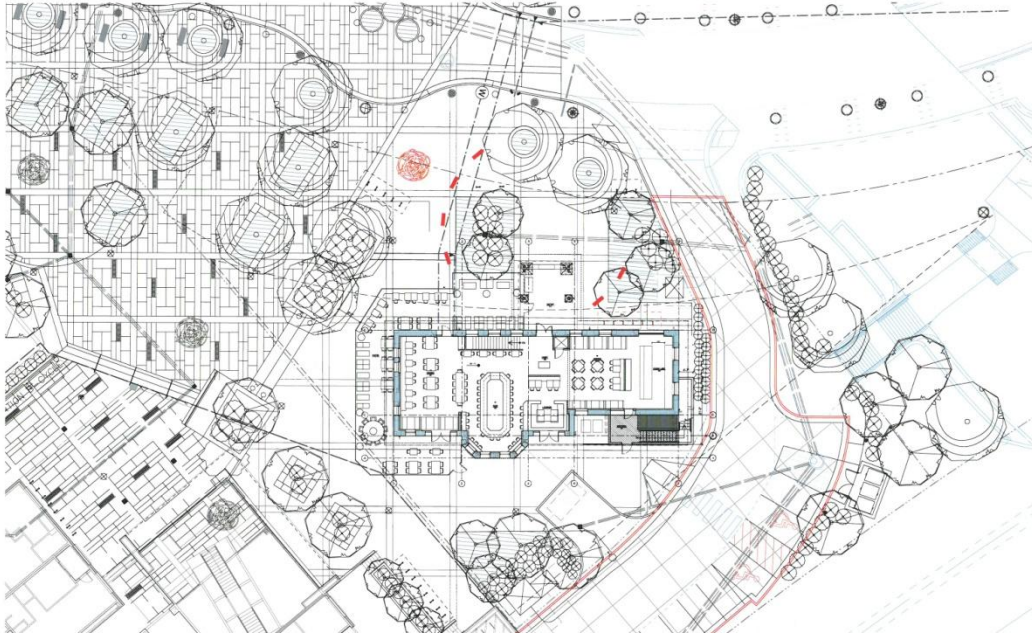


Figure 11. Proposed Site Plan, 2014.

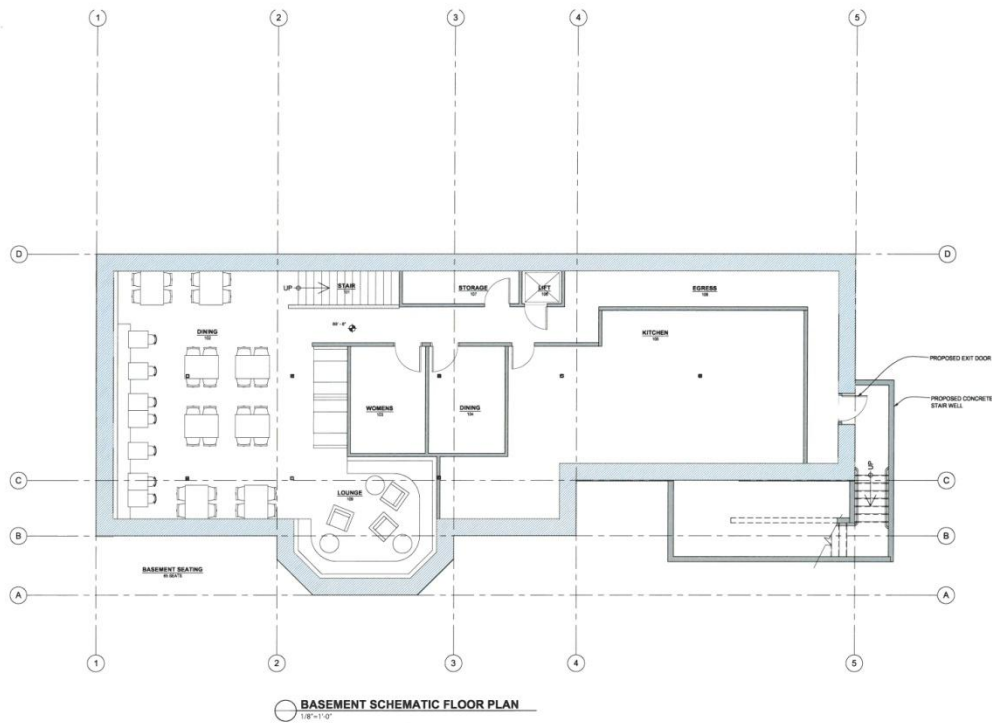


Figure 12. Basement floor plan showing proposed service area, 2014.



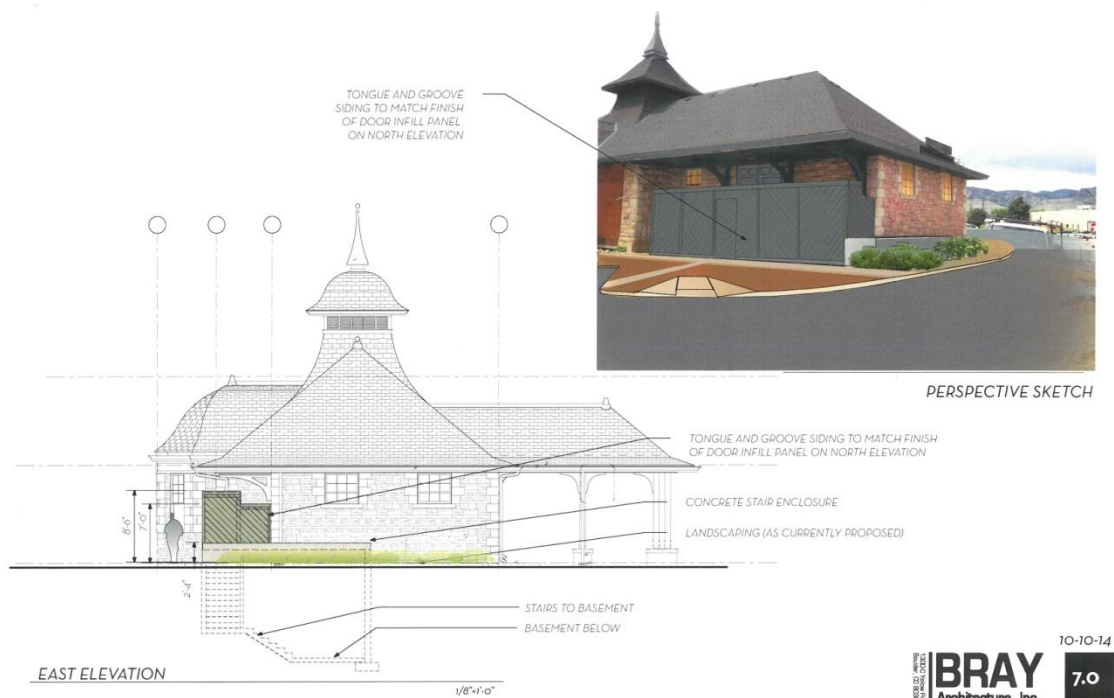


Figure 12. Proposed East Elevation and Perspective Sketch, 2014.

Plans also call for the rear arched trackside entrances that originally functioned as access to the segregated men's and women's "retiring rooms". Original drawings show these loggia's to be inset approximately 8 ft. from the exterior wall, and each to access the rooms via 4-panel, four light double-doors. A six light window is shown to have also been located on the inset wall in each loggia. This configuration was also shown in the 2011 submittal for the restoration of the building which was approved by the Ldrc. See Attachment D for a side-by-side view of the original and proposed treatment of the trackside arches.

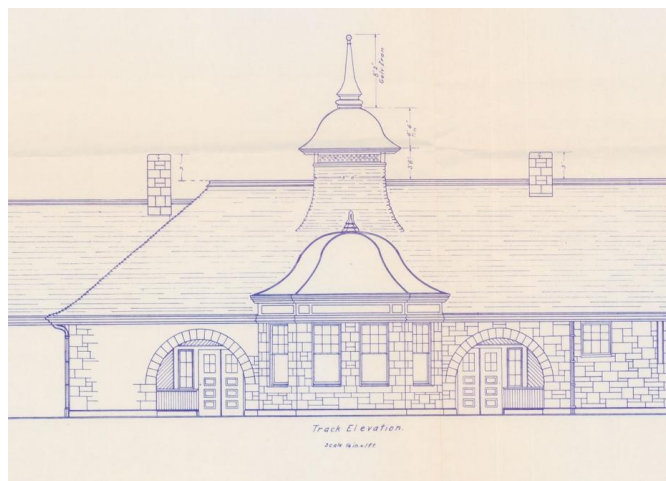


Figure 13. Portion of the original 1890 trackside elevation for the Depot

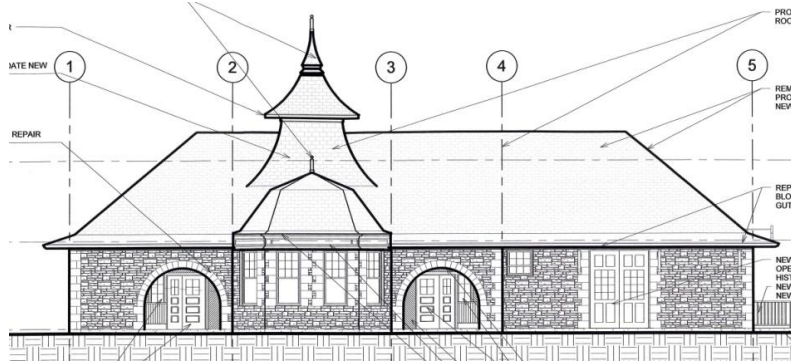


Figure 14. Approved 2011 LAC drawings, trackside

In order to provide more functional interior space for the proposed use as a restaurant, the applicant is requesting the loggia space inset approximately 1' from the exterior wall where originally the inset was approximately 8'. Likewise, the applicant is proposing the fenestrated inside each arch to be somewhat modified. Instead of the quarter light door, the request is for wider, three quarter ten light wood doors. The proposed configuration would also eliminate the six light window originally located in the loggias.

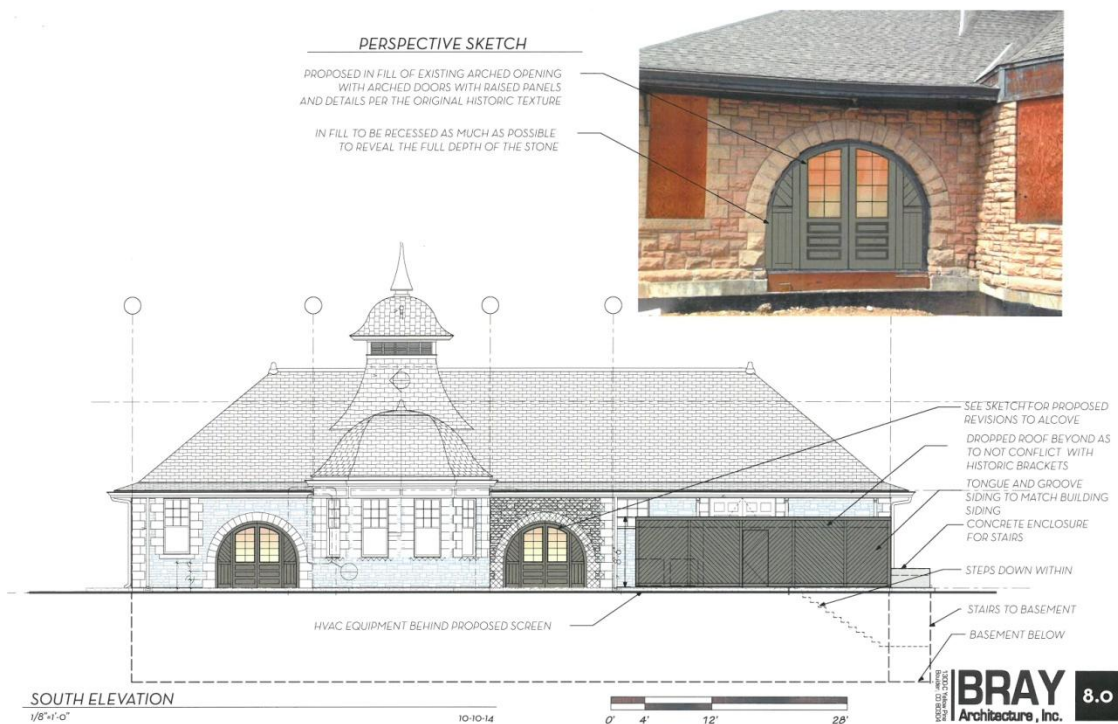


Figure 15. Current proposal South Elevation and Perspective Sketch, 2014.

## **CRITERIA FOR THE BOARD'S DECISION**

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Subsection 9-11-18(b) and (c), B.R.C. 1981, sets forth the standards the Landmarks Board must apply when reviewing a request for a Landmark Alteration Certificate.

(b) Neither the Landmarks Board nor the City Council shall approve a Landmark Alteration Certificate unless it meets the following conditions:

- (1) The proposed work preserves, enhances, or restores and does not damage or destroy the exterior architectural features of the landmark or the subject property within an historic district;
- (2) The proposed work does not adversely affect the special character or special historic, architectural, or aesthetic interest or value of the landmark and its site or the district;
- (3) The architectural style, arrangement, texture, color, arrangement of color, and materials used on existing and proposed constructions are compatible with the character of the existing landmark and its site or the historic district;
- (4) With respect to a proposal to demolish a building in an historic district, the proposed new construction to replace the building meets the requirements of paragraphs (b)(2) and (3) above.

(c) In determining whether to approve a landmark alteration certificate, the Landmarks Board shall consider the economic feasibility of alternatives, incorporation of energy-efficient design, and enhanced access for the disabled.

## **ANALYSIS**

*1. Does the proposed application preserve, enhance, or restore, and not damage or destroy the exterior architectural features of the landmark or the subject property within a historic district?*

Staff finds that the construction of the proposed addition will be generally compatible and consistent with the *General Design Guidelines* (see Design Guidelines Analysis section). Historically, this end of the trackside face of the building was a service entrance and the main entrance to the freight area of the building. Repurposing the building as a restaurant requires a service entrance and a location for mechanical equipment. Given the historic of the building and steps that have been taken to ensure reversibility of the addition, the proposed additions are appropriate.

Reconfiguring the, no longer extant, trackside loggias from the original design is also generally appropriate given that providing for 8' deep loggias on the back of the building is impractical and could be to the detriment of the long-term viability and condition of the depot. Staff considers that care should be taken to detail the archways and consideration might be given to inseting the arch walls at least 2' to provide more depth to better replicate this character defining feature of the building's trackside face.

*2. Does the proposed application adversely affect the special character or special historical, architectural, or aesthetic interest or value of the district?*

Staff finds that recommended conditions are met, the proposal will not adversely affect the special character or special historic, architectural, or aesthetic interest or value of the district because the proposed new garage will be generally compatible with the *General Design Guidelines* in terms of mass, scale, height, design and color (see Design Guidelines Analysis section).

*3. Is the architectural style, arrangement, texture, color, arrangement of color, and materials used on existing and proposed structures compatible with the character of the historic district?*

Staff considers that provided the recommended conditions are met the architectural style, arrangement, texture, color, arrangement of color, and materials of the proposed garage to be compatible with the contributing house on the property and it will be generally compatible with the character of the historic district (see Design Guidelines Analysis section).

*4. Does the proposal to demolish the building within the Mapleton Hill Historic District and the proposed new construction to replace the proposed demolished building meet the requirements of paragraphs 9-11-18(b)(2), 9-11-18(b)(3) and 9-11-18(b)(4) of this section?*

Not applicable.

#### **ANALYSIS:**

The Historic Preservation Ordinance sets forth the standards the Landmarks Board must apply when reviewing a request for a Landmark Alteration Certificate. The Board has adopted the *General Design Guidelines* to help interpret the historic preservation ordinance. The following is an analysis of the proposed new construction with respect to relevant guidelines. Design guidelines are

intended to be used as an aid to appropriate design and not as a checklist of items for compliance.

The following is an analysis of the proposal's compliance with the appropriate sections of the *General Design Guidelines*.

## GENERAL DESIGN GUIDELINES

### ALTERATIONS TO CONTRIBUTING BUILDINGS, 3.0

<b>3.1</b>	<b>Roofs</b>		
	<i>The roof is one of the primary character-defining features of a historic building, and the repetition of similar roof types creates part of the visual consistency that defines a historic area. Alterations or additions to roofs must be given careful consideration to ensure that they do not compromise the integrity of the historic structure. Typical roof shapes are gabled or hipped. Shed roofs sometimes occur on historic additions and accessory buildings.</i>		
	<b>Guideline</b>	<b>Analysis</b>	<b>Meets Guideline?</b>
.1	<i>Maintain the roof form, slope, height, and orientation to the street.</i>	Roof form will be maintained; the addition is located under the overhanging eave and below the architectural brackets as to not damage this architectural feature.	Yes
.5	<i>Roof appurtenances such as swamp coolers, TV antennas, and satellite dishes should be installed so that they are not visible from the street and do not damage or obscure historic features.</i>	Mechanical equipment cannot be located on the roof, and is proposed to be screened at the north elevation.	Yes
<b>3.6</b>	<b>Exterior Materials: Walls, Siding, and Masonry</b>		
	<i>Brick, stone, horizontal wood-lapped siding, stucco, and wood shingles are common finish materials found in historic districts and on historic structures.</i>		
	<b>Guideline</b>	<b>Analysis</b>	<b>Meets Guideline?</b>
.2	<i>New finish materials should be compatible with, but not seek to replicate, original finish materials.</i>	Proposed addition is shown to be wood tongue-in-groove siding to match finish of door infill panel on the north elevation.	Yes
<b>3.7</b>	<b>Windows</b>		



	<i>Windows, the elements that surround them, and their relationship to one another are one of the most important character-defining elements of a historic structure and should be preserved. Improper or insensitive treatment of the windows on a historic structure can seriously detract from its architectural character. Windows on facades visible from public streets, particularly the front façade, are especially important.</i>		
	Guideline	Analysis	Meets Guideline?
.1	<i>Retain and preserve existing historic windows including their functional decorative features . . . In some cases, it might be appropriate to use window elements from the side or rear elevations to repair those on the front.</i>	Non-historic windows, installed in the 1970s, were removed prior to the Depot's relocation in 2008. No historic windows exist. Custom windows are to be built based on historic drawings and photographs (under a separate LAC).	Yes
<b>3.8</b>	<b>Doors</b>		
	<i>Front doors and primary entrances are among the most important elements of historic buildings. The original size and proportion of a front door, the details of the door, the door surround, and the placement of the door all contribute to the character of the entrance.</i>		
	Guideline	Analysis	Meets Guideline?
.1	<i>Whenever possible, retain and preserve all original doors and door openings...</i>	The applicant proposes to restore the two openings on the south elevation based on the original plans and historic photographs. The historic loggias no longer exist and these features are to be reconstructed. Staff considers depth of arched openings should be increased to at least 2' to provide depth to provide for archways to read more as loggias. Proposed door design differ from those on historic drawings and windows eliminated in each arch.	Maybe
.2	<i>If replacement is found to be appropriate, the replacement door should match the original as closely as possible...</i>	The original doors no long exist. Proposed door design differ from those on historic drawings and windows eliminated in each arch.	Maybe

## ADDITIONS TO HISTORIC BUILDINGS, 4.0.

<b>4.1</b>	<b>Protection of Historic Structures and Sites</b>		
	<i>The primary concern of the Landmarks Board in reviewing additions to historic structures is the protection of the existing structure and the character of the site and district.</i>		
	<b>Guideline</b>	<b>Analysis</b>	<b>Meets Guideline?</b>
.1	<i>Construct a new addition so that there is the least possible loss of historic fabric and so that the character-defining features of the historic building are not destroyed, damaged, or obscured.</i>	The addition is shown to be tucked under the existing eaves, and will be constructed in a manner that would be easily reversible, not causing damage to the historic building. The walls of the addition step down at the brackets to ensure visibility of this character-defining feature is maintained.	Yes
.2	<i>New additions should be constructed so that they may be removed in the future without damaging the historic structure.</i>	The applicant indicates that the addition will be constructed in a manner so that it could be removed in the future without damaging the historic masonry of the building	Yes
.3	<i>It is not appropriate to construct an addition that will detract from the overall historic character of the principal building and/or the site, or if it will require the removal of significant building elements or site features.</i>	The addition is shown to be tucked under the existing eaves, and will be constructed in a manner that would be easily reversible, not causing damage to the historic building. The walls of the addition step down at the brackets to ensure visibility and integrity of this character-defining feature is maintained.	Yes
<b>4.2</b>	<b>Compatibility with Historic Buildings</b>		
	<i>All additions should be discernible from the historic structure. When the original design is duplicated the historic evolution of the building becomes unclear. Instead, additions should be compatible with the historic architecture but clearly recognizable as new construction.</i>		
	<b>Guideline</b>	<b>Analysis</b>	<b>Meets Guideline?</b>
.1	<i>Distinguish an addition from the historic structure, but maintain visual continuity between the two. One common method is to step the</i>	The addition is proposed to have a tongue and groove wood finish, which would clearly distinguish the old from the new.	Yes

	<i>addition back and/or set it in slightly from the historic structure. Every project is different and successful designs may incorporate a variety of approaches.</i>		
.2	<i>Do not copy historic elements. Instead, interpret historic elements in simpler ways in the addition.</i>	The addition as proposed is simple and secondary to the historic building.	Yes
.3	<i>Additions should be simpler in detail than the original structure. An addition that exhibits a more ornate style or implies an earlier period of architecture than that of the original is inappropriate.</i>	Addition as proposed utilizes a simpler material (wood) and is simply detailed that is complimentary and of its time.	Yes
.4	<i>The architectural style of additions should not imitate the historic style but must be compatible with it. Contemporary style additions are possible, but require the utmost attention to these guidelines to be successful...</i>	The addition is simple and does not imitate the historic style of the Depot.	Yes
<b>4.3</b>	<b>Compatibility with Historic Structures</b>		
	<i>Introducing new construction that contrasts sharply with an existing historic structure or site detracts from the visual continuity that marks our historic district. While additions should be distinguishable from the historic structure, they must not contrast so sharply as to detract from the original building and/or the site. Additions should never overwhelm historic structures or the site, in mass, scale, or detailing.</i>		
	<b>Guideline</b>	<b>Analysis</b>	<b>Meets Guideline?</b>
.1	<i>An addition should be subordinate to the historic building, limited in size and scale so that it does not diminish or visually overpower the building.</i>	The addition as proposed is limited in size and scale, and located under the existing overhanging eaves. The addition will not overpower the building.	Yes
.2	<i>Design an addition to be compatible with the historic building in mass, scale, materials and color. For elevations visible from public streets, the relationship of solids to</i>	The addition will be compatible, as it is limited in scale and mass, and uses traditional material (wood) painted in a subdued color. The addition does not introduce new	Yes

	<i>voids in the exterior walls should also be compatible.</i>	voids/openings, retaining a simple character.	
.3	<i>Adding a partial or full story to the historic portion of a historic building is rarely appropriate.</i>	Full or partial story not proposed.	Yes
.4	<i>Reflect the original symmetry or asymmetry of the historic building.</i>	Addition will retain the building's symmetry.	Yes
.5	<i>Preserve the vertical and horizontal proportion of a building's mass.</i>	Addition will retain the building's horizontal proportion.	Yes

4.4	Compatibility with Historic Site and Setting		
	<i>Additions should be designed and located so that significant site features, including mature trees, are not lost or obscured. The size of the addition should not overpower the site or dramatically alter its historic character.</i>		
	Guideline	Analysis	Meets Guideline?
.1	<i>Design new additions so that the overall character of the site, site topography, character-defining site features and trees are retained.</i>	The addition is located at the northeast side of the building, its visibility mitigated through its simple design and location under the over-hanging eaves. Character-defining features of the site will not be impacted.	Yes
.2	<i>Locate new additions on an inconspicuous elevation of the historic building, generally the rear one. Locating an addition to the front of a structure is inappropriate because it obscures the historic facade of a building.</i>	The Depot does not have an inconspicuous elevation due to its prominence in the future Depot Plaza. The proposed location does not detract from the building or obscure character defining features.	

4.5	Key Building Elements		
	<i>Roofs, porches, dormers, windows and doors are some of the most important character-defining elements of any building. As such, they require extra attention to assure that they compliment the historic architecture. In addition to the guidelines below, refer also to Section 3.0 Alterations for related suggestions.</i>		
			Meets

	<b>Guideline</b>	<b>Analysis</b>	<b>Guideline?</b>
.1	<i>Maintain the dominant roofline and orientation of the roof form to the street.</i>	Proposed addition does not impact the roofline of the building.	Yes
.2	<i>Rooflines on additions should be lower than and secondary to the roofline of the original building.</i>	Roof of the addition is flat and lower than the original roofline. The addition is secondary to the original building.	Yes
.3	<i>The existing roof form, pitch, eave depth, and materials should be used for all additions.</i>	Staff considers that in this case, a flat roof has the lowest profile and the least impact on the historic character of the building.	Yes
.5	<i>Maintain the proportion, general style, and symmetry or asymmetry of the existing window patterns.</i>	Proposal partially obscures a door opening on the south elevation. Utilization of this existing opening prevents further modification of the historic building. Restoration of the alcoves on the south elevation is encouraged.	Yes
.6	<i>Use window shapes that are found on the historic building. Do not introduce odd-shaped windows such as octagonal, triangular, or diamond-shaped</i>	Windows not proposed on the addition.	Yes

Staff considers the proposed construction of an addition on the south and east elevation of the historic Depot to be consistent with the historic preservation ordinance and Sections 3 and 4 of the *General Design Guidelines*. The addition is shown to be tucked under the over-hanging eave and separated from the decorative brackets, a character-defining feature of the Depot. The addition is proposed to be clad in tongue and groove wood, a traditional material that is complimentary but distinct from the historic building. The addition will obscure an existing opening on the south elevation, however, this incorporation of an existing opening will prevent further modification of the building. The proposed restoration of the alcoves at the arches on the south elevation is based on the original plans and historic photographs and will celebrate the building's history as a train depot. The concrete enclosure on the east elevation is simple in detailing and material, and will not detract from the historic character of the building. Overall, the proposed addition represents a creative and modest addition that will allow the building to function in the next chapter of its history.



Staff considers that in terms of the archways, details should be reviewed at the Landmarks design review committee to ensure that the fenestration inside the arches is as accurate as possible. Likewise, staff considers that effort should be made to inset the arch walls at least 2' and that this details should be reviewed and approved by the landmark design review committee.

#### **PUBLIC COMMENT**

Staff has received no public comment regarding this case.

#### **FINDINGS:**

Provided the conditions outlined in the staff recommendation are met, staff recommends that the Landmarks Board approve the application and adopt the following findings:

1. The proposed new construction meets the standards in 9-11-18 of the Boulder Revised Code.
2. The proposed addition will not have an adverse effect on the value of the landmark property, as it will be generally compatible in terms of mass, scale, or orientation with the historic character of the building.
3. In terms of mass, scale, and detailing, the proposed addition will be generally consistent with Section 9-11-18 B.R.C., Sections 3 and 4 of the *General Design Guidelines*.

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#### **ATTACHMENTS:**

- A: Tax Assessors Card
- B: Photographs
- C: Plans and Application
- D: Original and Proposed Treatment of the Trackside Arches

## Tax Assessors Card

[illegible]







Tax Assessor Card Photo, 1960





*Photo 1: View of Northwest corner of Depot at current location, 2014.*



*Photo 2: View of Depot from bike path, construction of new bridge on left, 2014.*





*Photo 3: Southwest corner of Depot, 2013.*



*Photo 4: East elevation of Depot, 2008.*



*Photo 5: Relocation across 30<sup>th</sup> St., 2008.*



*Photo 6: Depot at 2275 30<sup>th</sup> St. location, 2006.*

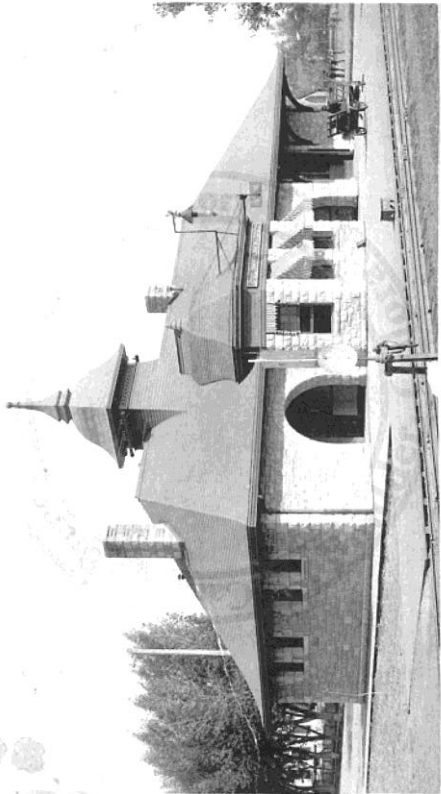




*Photo 7: Depot at 14<sup>th</sup> and Canyon. Original porte cochere on left demolished c. 1940s.  
Photo taken c. 1900.*



*Photo 8: Photo of Depot at 14<sup>th</sup> and Canyon ca. 1951-1958.*



**Index: Drawing Exhibits**

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APPLICATION FOR LANDMARKS ALTERATION CERTIFICATE

BOULDER HISTORIC DEPOT  
10-10-14

## The Depot

### Landmark alteration certification request

#### Landmarks Board,

We are pleased to offer the attached submittal for the proposed new restaurant tenant at the Depot. Dionysus Restaurant group has multiple locations in Denver and in the resort areas of Colorado and has been researching this project over the last year. The adaptation of the depot into a restaurant will provide a vibrant use that allows the public to once again enjoy this cherished landmark.

#### Context

The Historic Train Station building has been relocated several times over the years it is now in its permanent home as a centerpiece to a public plaza within the Transit Village. The plaza is currently under construction, and the Historic Train Station building is under lease with the intent to be occupied by a family-style restaurant.

The Train Station building is no longer intended to provide a municipal function (as in its original use) but will now house a commercial use.

The building is roughly 2,250 s.f. on the main level and 2,400 s.f. in a basement, (which was not part of the original train station) is currently under construction. As part of the preparation for the buildings intended use, as well as rehabilitating the existing structure, a set of construction documents were prepared by SHE architects defining the scope of work related to the historic renovation. The intent of the documents was to allow the necessary re-construction of much of the buildings detail to be as close as possible to the original buildings design.

The building has obvious historic and cultural significance to the city of Boulder and we realize and fully appreciate what this building means to the community. Our intent is to be allowed to add subtle additions and modifications which respect the original architectural detail, materials, and style, while adding key features we feel are necessary in addressing key programmatic requirements required by a restaurant use within this building.

In the original building use there was not a distinct "front" and "back" of the building as both sides were equally activated. In its new use however there will be a distinct front and back, and the back will necessarily become the service-entry for the many goods and services that come and go the building on a daily/hourly basis.

Most projects change over time and while the new use is appropriate for the historic building, it does provide some challenges. The footprint is restricted in geometry with limited width that is inherent in this building type, restaurants all have requirements of mechanical, shipping and receiving that need to be accounted for in the design of these structures. Below are the modification we propose to the previously approved LAC:

#### Screened Mechanical and Service entry:

The previous approved LAC attempted to address these needs by providing an exterior stair to the basement at the north side of the building. This however is inadequate, as it needs to be enclosed to meet health codes and to be reasonably usable during the winter. A second required stair is also be need for the basement which restricts the floor plan inside. There is no provision what so ever in the previous LAC to allow for mechanical required for a restaurant. The proposed screen would harbor the mechanical at the east side away from visual exposure, which would detract from the character of the building.

Our intent is to construct this enclosure similar to the diagonal tongue and groove siding utilized in the existing structure as a secondary material to infill areas defined by the stone. This will be constructed in way that does not impact the existing structure and will easily removable if desired at some point in the future.

#### Arched windows at the east façade:

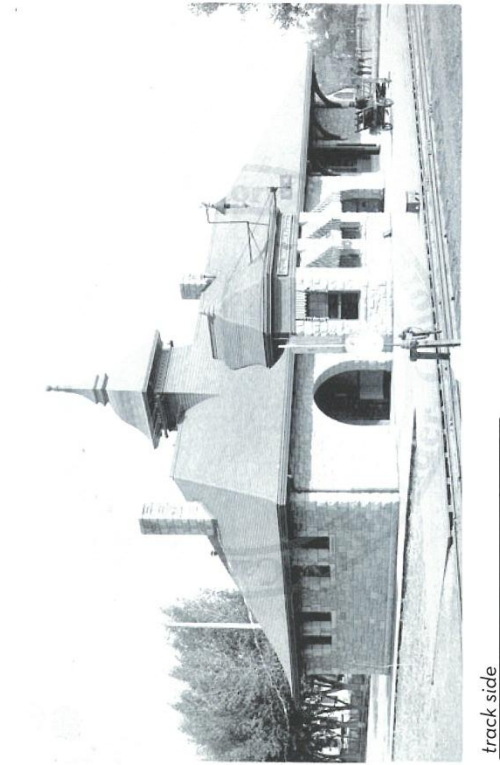
The LAC also proposed recreating the previous restroom vestibules from the original building. These vestibules would become an attractive nuisance for trash and other unwanted issues and are not large enough for any other practical function. The proposed elimination of these allows the public to interact with the arch features of the building and also opens the building up to the east elevation which would otherwise be cut off.

#### Landmarks Criteria:

1. *The proposed work preserves, enhances, or restores and does not damage or destroy the exterior architectural features of the landmark.* The proposed addition preserves the restored building and does not alter any historic fabric of the building. The proposed addition will be constructed in methods that are easily removable.
2. *The proposed work does not adversely affect the special character or special historical, architectural, or aesthetic interest or value of the landmark.* The addition is intentional tucked to the eastern side out of the primary exposures provided by the site. The addition is also the primary way to ensure that the character of the building is maintained by screening the mechanical and the service to the building.
3. *The architectural style, arrangement, texture, color, arrangement of color, and materials used on existing and proposed structures are compatible with the character of the existing landmark and its site or the historic district.* We have selected simple materials and methods of wood construction that are sympathetic with the era of the building.

James A. Bray,  
Architect

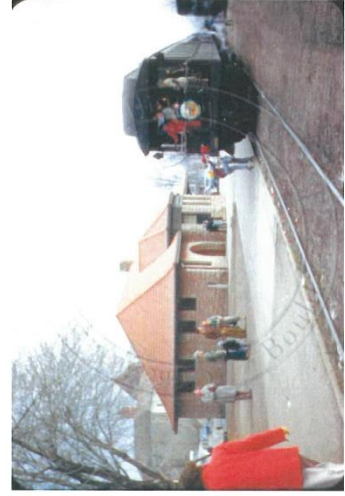




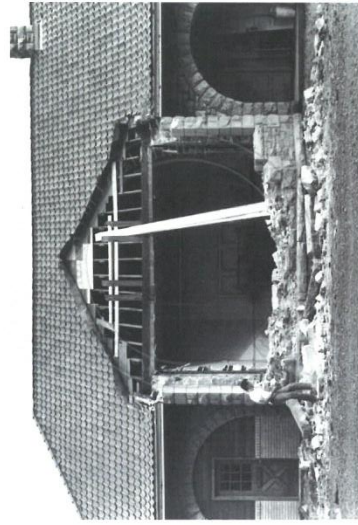
track side



track side, 1930



track side, 1950

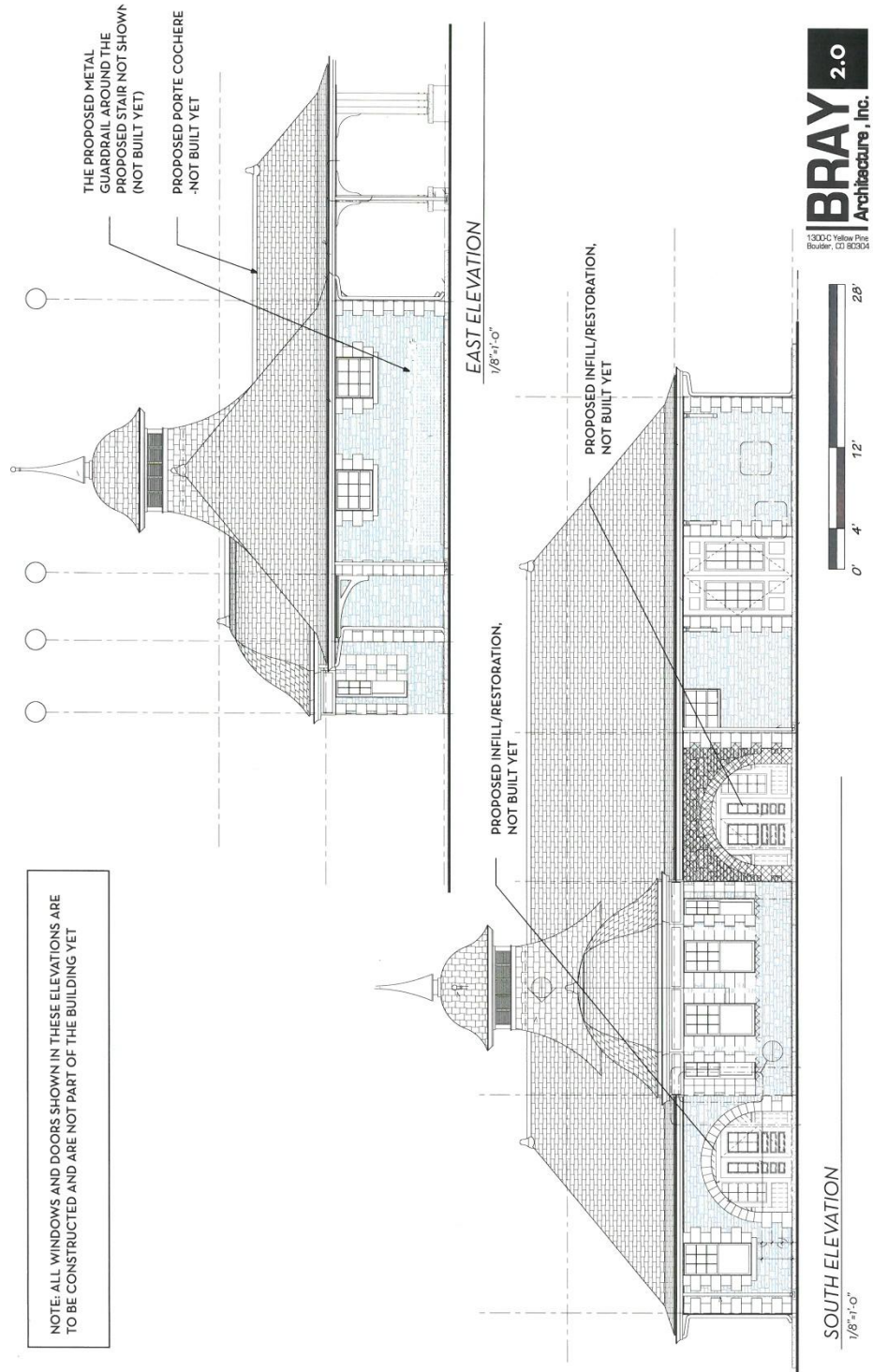


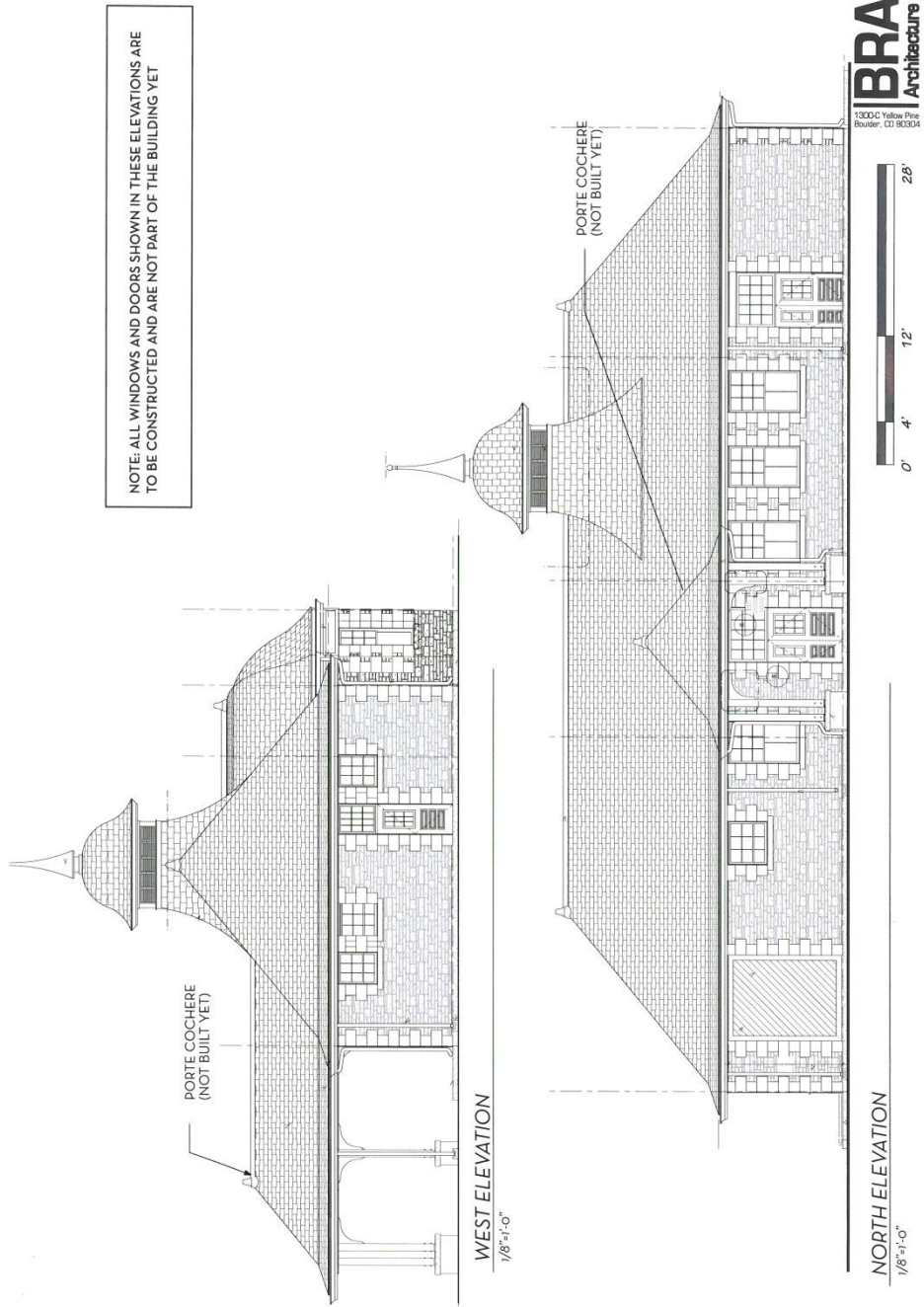
The first move in 1960

10-10-14

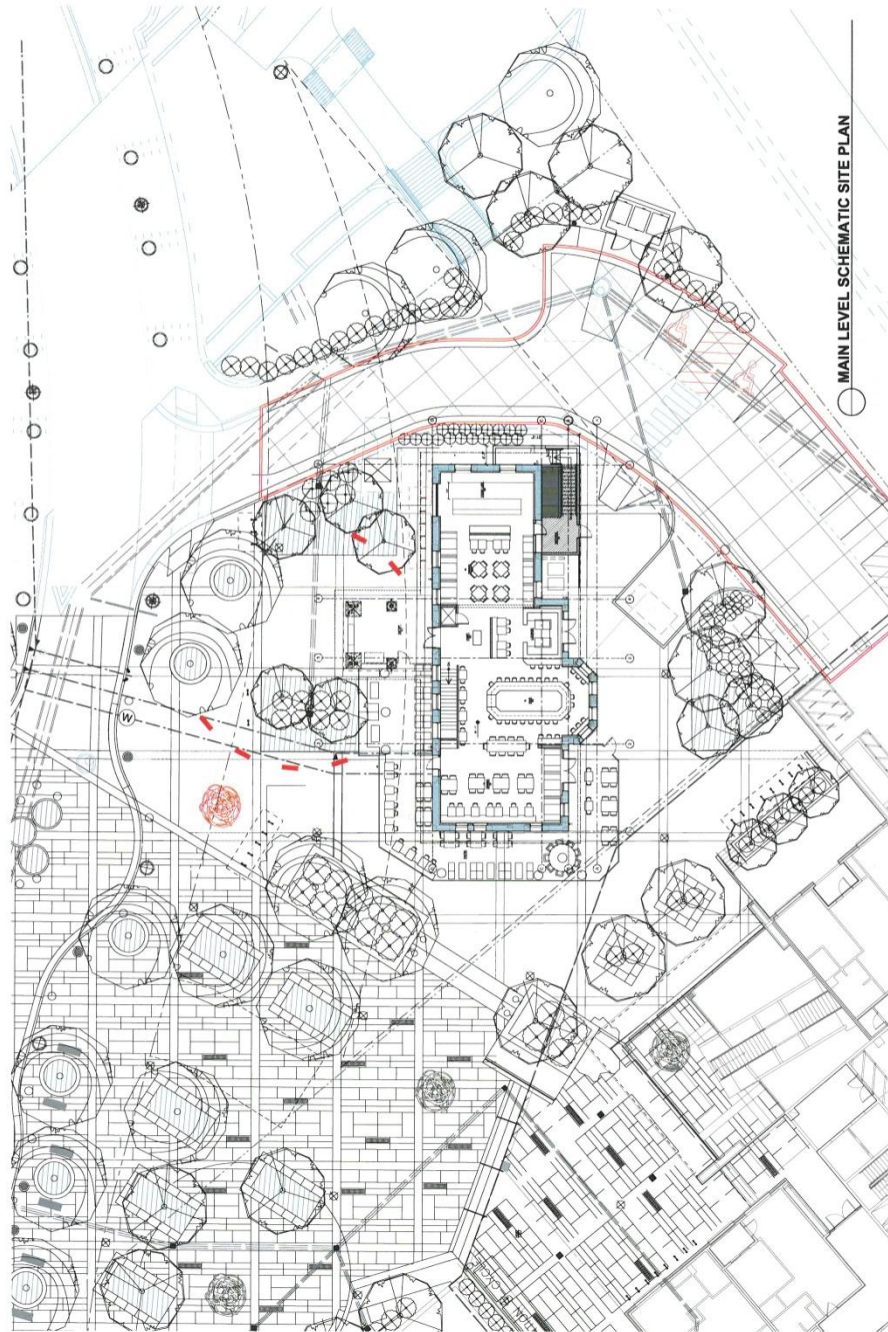
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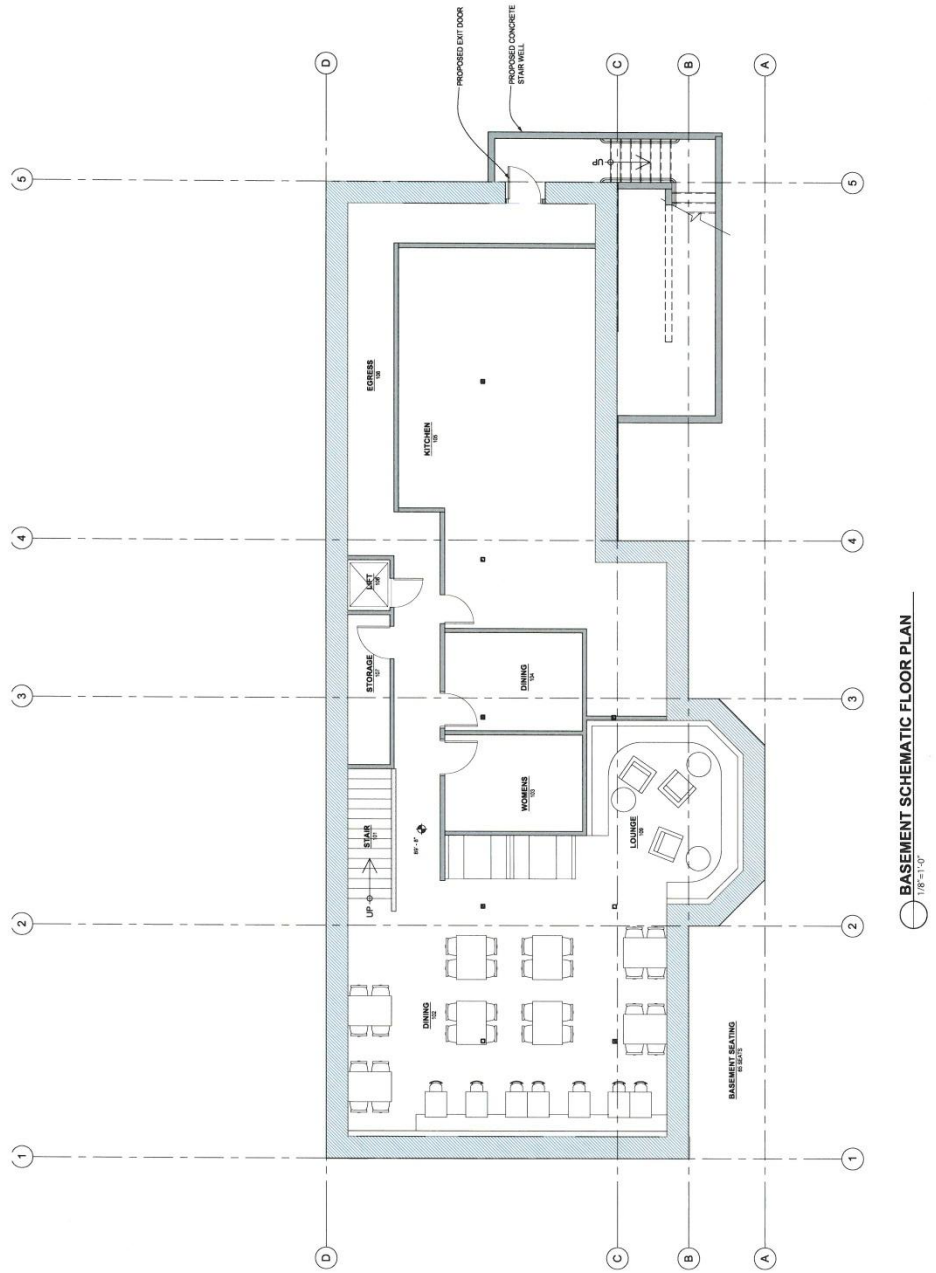
**BRAY**  
Architecture, Inc.  
1300C Yellow Pine  
Boulder, CO 80504











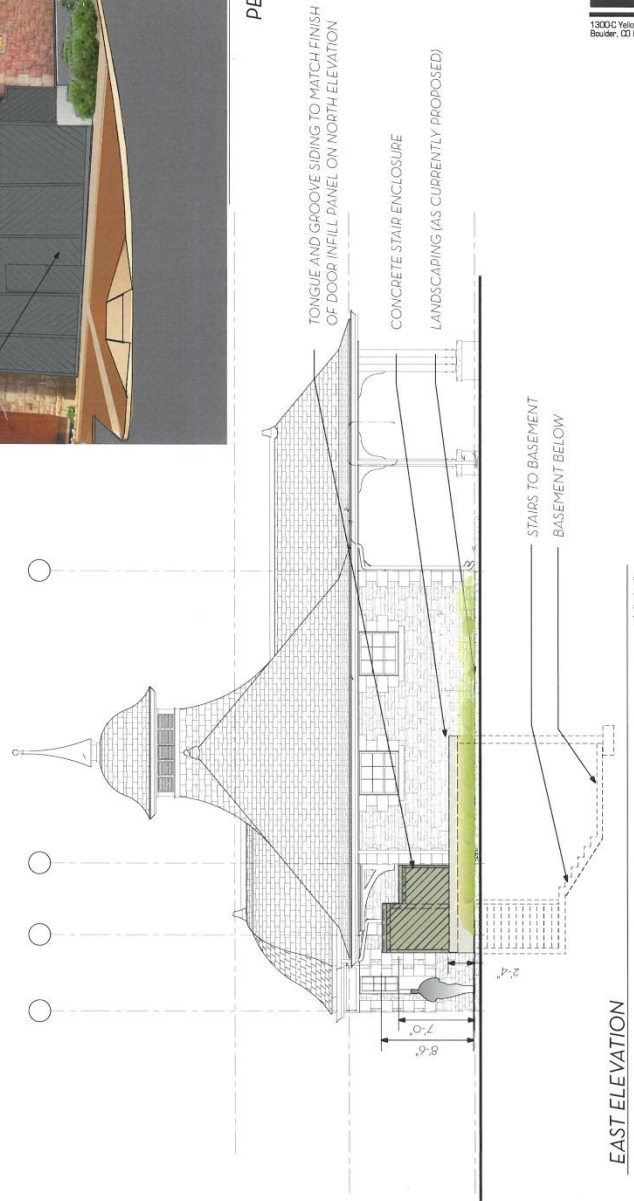






PERSPECTIVE SKETCH

TONGUE AND GROOVE  
SIDING TO MATCH FINISH  
OF DOOR INFILL PANEL  
ON NORTH ELEVATION



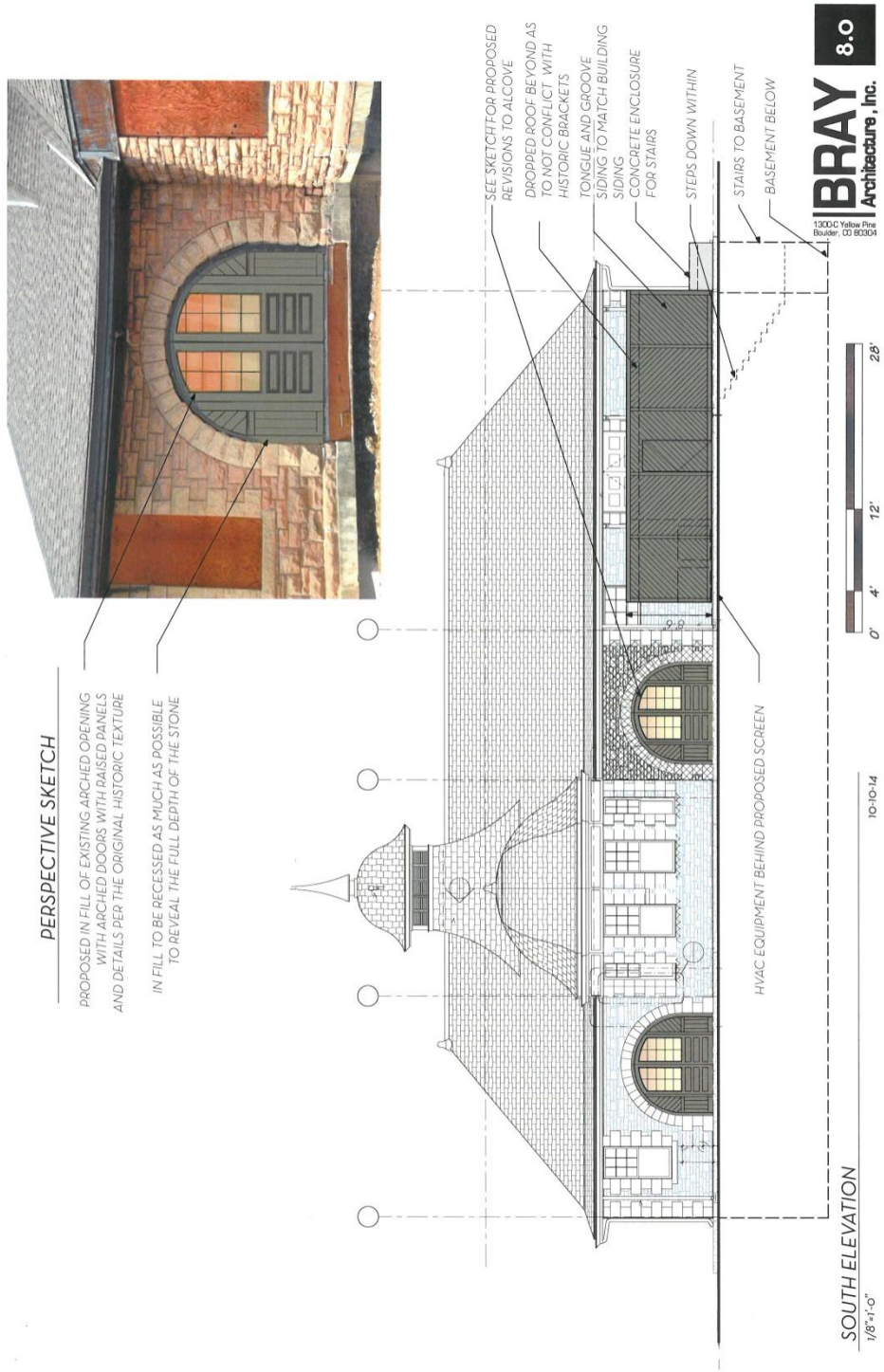
EAST ELEVATION

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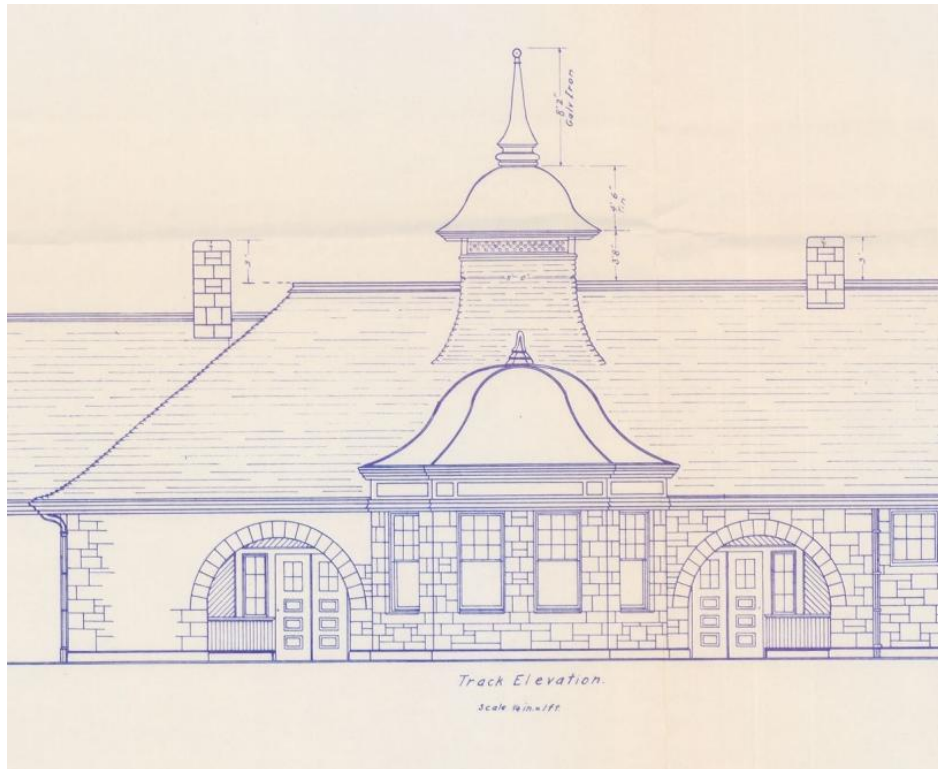
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1300C Yellow Pine  
Boulder, CO 80504

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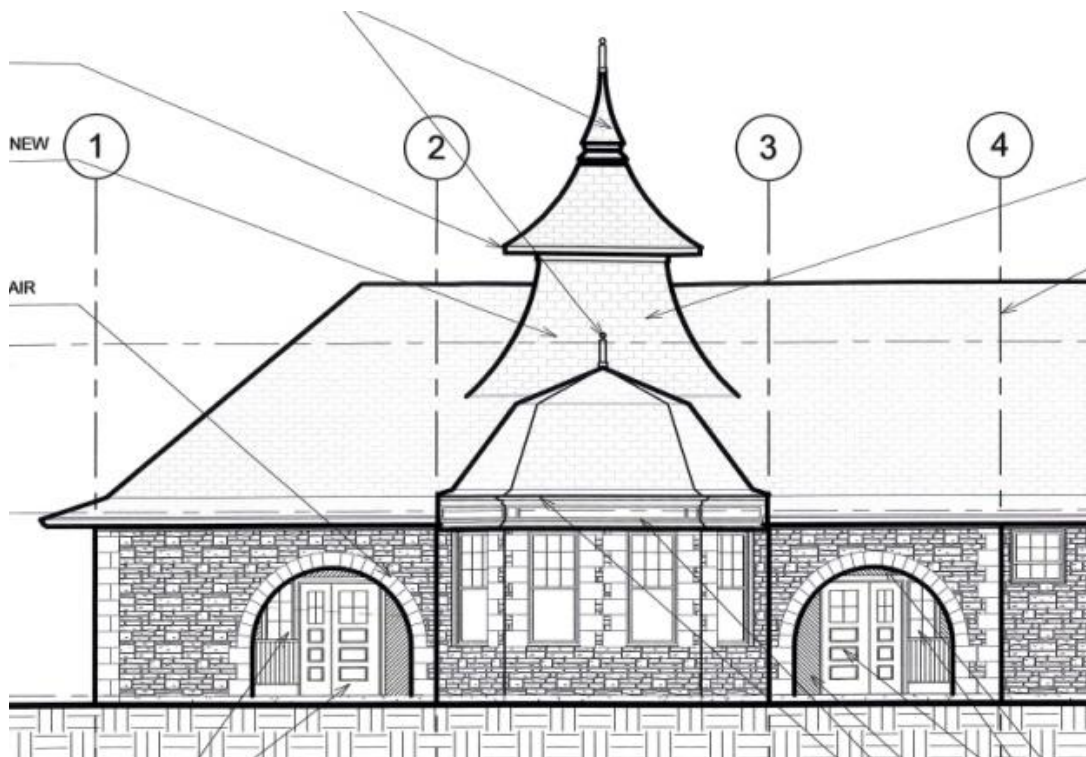




Attachment D: Original and Proposed Treatment of the Trackside Arches



*Original 1890 trackside elevation for the Depot*



*Figure 14. Approved 2011 LAC drawings, trackside*